



VIRTUAL DESIGN PUBLIC HEARING

AUGUST 18, 2021

FOR THE PROPOSED

**TRAFFIC SIGNAL UPGRADES AND RELATED WORK
ALONG HIGH & MAPLE STREETS,
FROM SOUTH STREET TO LYMAN STREET
Project Management**

IN THE CITY OF HOLYOKE, MASSACHUSETTS

**COMMONWEALTH OF MASSACHUSETTS
MASSACHUSETTS DEPARTMENT OF TRANSPORTATION
HIGHWAY DIVISION**

**JONATHAN GULLIVER
HIGHWAY ADMINISTRATOR**

**CARRIE E. LAVALLEE, P.E.
ACTING CHIEF ENGINEER**

THE COMMONWEALTH OF MASSACHUSETTS
MASSACHUSETTS DEPARTMENT OF TRANSPORTATION – HIGHWAY DIVISION
NOTICE OF A PUBLIC HEARING
Project File No. 606450

A Live Virtual Design Public Hearing will be hosted on the MassDOT website below to present the design for the proposed Traffic Signal Upgrades and Related Work along High & Maple Streets Project in Holyoke, MA.

WHEN: 6:30 PM, Wednesday August 18, 2021

PURPOSE: The purpose of this hearing is to provide the public with the opportunity to become fully acquainted with the proposed High & Maple Streets Traffic Signal Upgrades project. All views and comments submitted in response to the hearing will be reviewed and considered to the maximum extent possible.

PROPOSAL: The proposed project consists of traffic signal upgrades at 8 intersections along each of High and Maple Streets between (south of) Resnic Boulevard and Lyman Street (a distance of approximately one mile). The project also includes American with Disabilities Act (ADA) compliance access ramps at all intersections, and new signage, revisions to lane use, on-street parking and pavement markings for on-road bicycle facilities between the intersections and within the project limits. Bicycle accommodations consisting of one 5-foot wide bicycle lane (with 2 to 7 feet buffer) is typically provided (consistent with the one-way traffic direction) on each street in accordance with applicable design guides.

A secure right-of-way is necessary for this project. Acquisitions in fee and permanent or temporary easements may be required. The city of Holyoke is responsible for acquiring all needed rights in private or public lands. MassDOT's policy concerning land acquisitions will be presented in the hearing.

Project inquiries, written statements and other exhibits regarding the proposed undertaking may be submitted to Carrie E. Lavallee, P.E., Acting Chief Engineer, via e-mail to MassDOTProjectManagement@dot.state.ma.us or via US Mail to Suite 6340, 10 Park Plaza, Boston, MA 02116, Attention: Project Management, Project File No. 606450. Statements and exhibits intended for inclusion in the public hearing transcript must be emailed or postmarked no later than ten (10) business days after the hearing is posted to the MassDOT website listed below.

This hearing is accessible to people with disabilities. MassDOT provides reasonable accommodations and/or language assistance free of charge upon request (e.g., interpreters in American Sign Language and languages other than English, live captioning, videos, assistive listening devices and alternate material formats), as available. For accommodation or language assistance, please contact MassDOT's Chief Diversity and Civil Rights Officer by phone (857-368-8580), TTD/TTY at (857) 266-0603, fax (857) 368-0602 or by email (MassDOT.CivilRights@dot.state.ma.us). Requests should be made as soon as possible prior to the meeting, and for more difficult to arrange services including sign-language, CART or language translation or interpretation, requests should be made at least ten business days before the hearing.

This Live Virtual Design Public Hearing or a cancellation announcement will be hosted on the internet at www.mass.gov/massdot-highway-design-public-hearings.

JONATHAN GULLIVER
HIGHWAY ADMINISTRATOR

CARRIE E. LAVALLEE, P.E.
ACTING CHIEF ENGINEER



Dear Concerned Citizen:

The Massachusetts Department of Transportation (MassDOT) is committed to building and maintaining a transportation infrastructure that is both safe and efficient for all who use our roadways, bridges, bicycle facilities and pedestrian paths, while maintaining the integrity of the environment.

As part of the design process for this project, we are conducting this public hearing to explain the proposed improvements, listen to your comments and answer any questions you may have. At the conclusion of the hearing, MassDOT will review all of your comments and, where feasible, incorporate them into the design of the project.

We recognize that road and bridge construction can create inconveniences for the public. MassDOT places a great deal of emphasis on minimizing the temporary disruptive effects of construction.

MassDOT encourages input from local communities and values your opinions. Please be assured that we will undertake no project without addressing the concerns of the community.

Sincerely,

Carrie E. Lavallee, P.E.
Acting Chief Engineer

WHAT IS A PUBLIC HEARING?

WHY A PUBLIC HEARING?

To provide an assured method whereby the Commonwealth of Massachusetts can furnish to the public information concerning the State's highway construction proposals, and to afford every interested resident of the area an opportunity to be heard on any proposed project. At the same time, the hearings afford the Commonwealth an additional opportunity to receive information from local sources which would be of value to the State in making its final decisions to what design should be advanced for development.

WHY NOT A VOTE ON HIGHWAY PLANS?

The hearings are not intended to be a popular referendum for the purpose of determining the nature of a proposed improvement by a majority of those present. They do not relieve the duly constituted officials of a State highway department of the necessity for making decisions in State highway matters for which they are charged with full responsibility.

WHAT DOES A PUBLIC HEARING ACCOMPLISH?

It is designed to ensure the opportunity for, or the availability of, a forum to provide factual information which is pertinent to the determination of the final alternative considered by the state to best serve the public interest, and on which improvement projects are proposed to be undertaken.

It is important that the people of the area express their views in regard to the proposal being presented, so that views can be properly recorded in the minutes of the meeting. These minutes will be carefully studied and taken into consideration in the determination of the final design.

RIGHT OF WAY CONCERNS

Your Municipality is responsible for securing the right of way for this project. If your property, or a portion of it, must be acquired by the Municipality for highway purposes in the interest of all people of the Commonwealth, your rights are fully protected under the law. Briefly, here are some of the answers to questions you might ask.

1. REASON FOR PROJECT

The completion of this project will serve local needs. The proposed enhancement will also be in the interest of others in the greater community, and provide for the public good.

2. WHO CONTACTS ME?

Representatives of the municipality have already contacted or will contact you. They will explain the procedures used in acquiring any necessary rights in land.

3. WHAT ABOUT DONATIONS? WHAT IS A RIGHT OF ENTRY?

Town officials will often seek donations, of parcels, where permanent rights are required. This procedure will minimize the acquisition cost for your community.

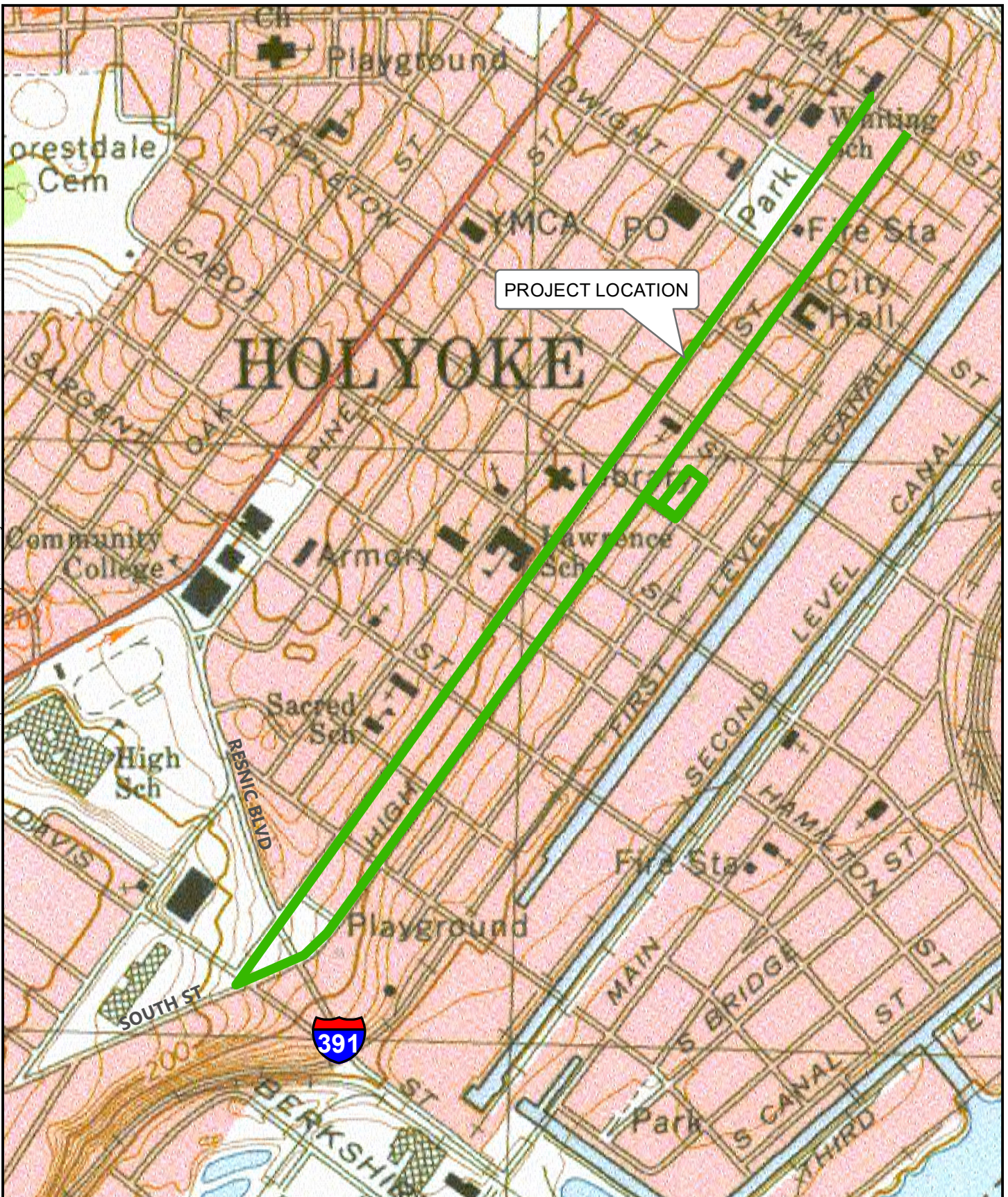
A Right of Entry is a document that is signed by the owner. It allows the Contractor to perform certain types of work on the owner's land. The work is usually minor in nature and frequently consists of loaming/seeding behind sidewalks, new driveway apron work, grading/sloping, and wetland protection, etc. The rights granted are temporary in nature.

4. WHAT IS A FAIR PRICE FOR THE ACQUIRED PARCELS?

In the event that donations are not considered, or completed, every effort will be made to ensure that an equitable value is awarded. Municipal and/or outside appraisers will complete an appraisal. Consideration is given to the type of rights needed, whether in fee, permanent or temporary easements. The appraisal will be the basis for arriving at a fair price (for damages that result).

5. MUST I ACCEPT THE MUNICIPALITY OFFER?

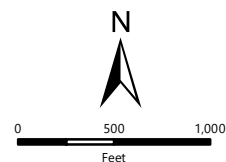
No, if the owner feels that the offer is not fair the owner may petition the courts. This action does not stop or delay the acquisition. The action must occur within 3 years. The owner(s) may be paid pro tanto (for the time being). The pro tanto payment will not prejudice the court's final decision.



1350 MAIN STREET
SUITE 1012
SPRINGFIELD, MA 01103
413.241.6920

Location Map

Traffic Signal Upgrades along High & Maple Streets
Holyoke, MA



SCALE 1" = 1,000'

DATE 7/23/2021

606450

PROJ. NO.

FIG. 1

PROPOSED IMPROVEMENTS

The High and Maple Streets traffic signals will be upgraded in order to improve traffic operation and safety for various transportation modes including motorists, transit, pedestrians, and bicyclists. High Street and Maple Street are parallel one-way streets (with traffic in northerly and southerly directions, respectively) and part of a grid network of streets covering the City's downtown core. The project limits along High and Maple Streets span approximately one mile from the intersection of South Street to the intersection with Lyman Street. The Holyoke Transportation Center, a major intercity bus terminal is located just north of Dwight Street along Maple Street. This facility is the City's transportation hub, which includes a bus station for the Pioneer Valley Transit Authority (PVTa).

The project includes replacing the functionally obsolete traffic signals at 16 intersections; eight (8) along High Street, from South to Dwight Streets and eight (8) on Maple Street, from South to Lyman Streets. New signals will include safety features such as accessible pedestrian signal (APS) push buttons, countdown pedestrian signal heads, and lead pedestrian intervals. Vehicular improvements will include new far-side signal heads with retroreflective backplates, new mast arms and controllers, video detection, emergency pre-emption and transit signal priority during off-peak hours.

The pedestrian curb ramps at signalized intersections will be reconstructed in compliance with American with Disabilities Act (ADA) standards. One 7-foot wide on-road bicycle lane and buffer, consistent with the one-way traffic direction will be provided in accordance with the applicable design guides. Two-stage bicycle turn box markings are proposed at signalized intersections to facilitate right-turn movements.

High Street

The proposed improvements for High Street include new pavement markings and signage to incorporate a bike lane that will be buffered at minimum of 2 feet from the adjacent travel lane by pavement markings. The bike lane switches from the right (east) side to the left (west) side of the street north of the Jackson Street intersection.

On-street parking will be retained along the sidewalk and to the left of bike lane north of Jackson Street. On-street parking will also be retained on the right side of the street with new pavement markings to designate zones for bus pull-offs. This configuration of bike lane and parking lane maintains the required 24 feet width for fire apparatus.

The existing pavement surface will be milled and overlaid between South Street and Appleton Street (Route 141) and sidewalks reconstructed to the ADA standards. The existing pavement and sidewalks between Appleton and Lyman Streets intersections will be retained. The existing sidewalk bump-outs at the intersections, street trees, decorative lighting, street furniture and bus shelters will be maintained.

Maple Street

The proposed improvements for Maple Street include new pavement markings and signage to incorporate a bike lane. The bike lane switches from the left (east) side to the right (west) side of the street south of the Jackson Street intersection and up to the project limit at South Street.

To accommodate the new bike lane (and its minimum 2-foot wide buffer) and maintain the existing travel lanes, the on-street parking will be eliminated along the left (east) side of Maple Street. From Jackson Street to Lyman Street, on-street parking will be retained on the right (west) side of the street with pavement markings that designate zones for bus pull-offs. Existing sidewalk bump-outs will be reduced to provide 2-foot wide minimum buffer between new bike lane and the adjacent travel lane.

While geometric changes to Maple Street are not proposed, a travel lane will be removed in front of the Holyoke Transportation Center (between Hamden and Dwight Streets) to accommodate a separated bike facility consistent with the rest of the project corridor.

The Maple Street pavement milling and overlay, and sidewalk and wheelchair accessible ramp reconstruction are limited to the intersections, and the existing pavement surface and sidewalks will be retained in between.

Utilities & Temporary Traffic Management During Construction

Utility work will be limited to relocation of the decorative streetlights that are at/near new wheelchair ramps and adjustment of the (utility) structures rim (e.g., manhole covers) level as needed in the proposed pavement resurfacing and/or sidewalks reconstruction areas.

Construction is currently anticipated to take about 2 years. At minimum one travel lane will be maintained on each street during construction. Temporary traffic signals will likely be needed while the existing signal equipment are being removed and new equipment are being installed.

Temporary pedestrian ramps, and signage and barricades will be installed for sidewalk closures, and temporary pavement markings will be placed to define travel lane limits/reduction. Portable message signs will be placed at project limits to alert the public to anticipated construction activities and/or lane closures.

THE COMMONWEALTH OF MASSACHUSETTS
MASSACHUSETTS DEPARTMENT OF TRANSPORTATION
HIGHWAY DIVISION

FEDERAL AID PROJECT

Holyoke, MA

Traffic Signal Upgrades and Related Work along High & Maple Streets,
from South of Resnic Boulevard to Lyman Street
Project File No. 606450

This sheet is provided for your comments. Your input is solicited and appreciated. You may e-mail comments directly to MassDOTProjectManagement@dot.state.ma.us or you may mail this sheet with any additional comments to:

Carrie E. Lavallee, P.E., Acting Chief Engineer
MassDOT – Highway Division
10 Park Plaza, Boston, MA 02116-3973
Attn: Project Management

Statements and exhibits intended for inclusion in the public hearing transcript must be emailed or postmarked no later than ten (10) business days after the hearing.

PLEASE TYPE OR PRINT LEGIBLY.

Name: _____ Title: _____

Organization: _____

Address: _____

Please Fold and Tape

Please Place
Appropriate
Postage Here

Carrie E. Lavallee, P.E.
Acting Chief Engineer
MassDOT – Highway Division
10 Park Plaza
Boston, MA 02116-3973

RE: Virtual Public Hearing
Traffic Signal Upgrades and Related Work along High & Maple Streets
From South Street to Lyman Street
HOLYOKE
Project File No. 606450
Attn: Project Management