



DESIGN PUBLIC HEARING

AUGUST 28, 2019

AT

FIRE STATION, 205 NORFOLK ROAD

SOUTHFIELD, MASSACHUSETTS

6:30 PM

FOR THE PROPOSED

Bridge Reconstruction/Rehab Project

Project No. 608645

Campbell Falls Rd over Whiting River, Bridge No. N-08-006

Bridge Project Management

IN THE TOWN OF NEW MARLBOROUGH, MASSACHUSETTS

COMMONWEALTH OF MASSACHUSETTS

MASSACHUSETTS DEPARTMENT OF TRANSPORTATION

HIGHWAY DIVISION

JONATHAN GULLIVER

HIGHWAY ADMINISTRATOR

PATRICIA A. LEAVENWORTH, P.E.

CHIEF ENGINEER

THE COMMONWEALTH OF MASSACHUSETTS

MASSACHUSETTS DEPARTMENT OF TRANSPORTATION – HIGHWAY DIVISION

NOTICE OF A PUBLIC HEARING
Project File No. 608645

A Design Public Hearing will be held by MassDOT to discuss the project that proposes to restore/rehabilitate one bridge in the Town of New Marlborough, MA; Campbell Falls Road over the Whiting River.

WHERE: New Marlborough Fire Station
205 Norfolk Road
Southfield, MA 01259
WHEN: Wednesday, August 28 2019 @ 6:30 PM

PURPOSE: The purpose of this hearing is to provide the public with the opportunity to become fully acquainted with the bridge rehabilitation project in New Marlborough. All views and comments made at the hearing will be reviewed and considered to the maximum extent possible.

PROPOSAL: The proposed project consists of the bridge reconstruction/rehabilitation of the bridge structure that was constructed in 1938. The roadway surface, bridge railings, concrete deck and steel beams for each bridge will be replaced. The new structures will consist of steel beams with composite reinforced concrete decks.

The 42' long single span Campbell Falls Road bridge will increase the width of the existing bridge to 16' curb to curb roadway width to match the approaches, and will consist of a single 16' shared travel lane. The existing abutments will be reused with modifications.

The bridge will be closed during construction and traffic will be detoured. Proposed approach roadway work includes the construction of full depth asphalt pavement and roadway resurfacing for approximately 175 feet on the west approach and 150 feet on the east approach of the bridge. There will be no sidewalks on the proposed bridge or approach roadway.

There are no Right of Way impacts anticipated on this project.

Written views received by MassDOT subsequent to the date of this notice and up to five (5) days prior to the date of the hearing shall be displayed for public inspection and copying at the time and date listed above. Plans will be on display one-half hour before the hearing begins, with an engineer in attendance to answer questions regarding this project. A project handout will be made available on the MassDOT website listed below.

Written statements and other exhibits in place of, or in addition to, oral statements made at the Public Hearing regarding the proposed undertaking are to be submitted to Patricia A. Leavenworth, P.E., Chief Engineer, MassDOT, 10 Park Plaza, Boston, MA 02116, Attention: Bridge Project Management Section, Project File No. 608645. Such submissions will also be accepted at the hearing. Mailed statements and exhibits intended for inclusion in the public hearing transcript must be postmarked within ten (10) business days of this Public Hearing. Project inquiries may be emailed to dot.feedback.highway@state.ma.us

This location is accessible to people with disabilities. MassDOT provides reasonable accommodations and/or language assistance free of charge upon request (including but not limited to interpreters in American Sign Language and languages other than English, open or closed captioning for videos, assistive listening devices and alternate material formats, such as audio tapes, Braille and large print), as available. For accommodation or language assistance, please contact MassDOT's Chief Diversity and Civil Rights Officer by phone (857-368-8580), fax (857-368-0602), TTD/TTY (857-368-0603) or by email (MassDOT.CivilRights@dot.state.ma.us). Requests should be made as soon as possible prior to the hearing, and for more difficult to arrange services including sign-language, CART or language translation or interpretation, requests should be made at least ten (10) business days before the hearing.

In case of inclement weather, hearing cancellation announcements will be posted on the internet at <http://www.massdot.state.ma.us/Highway/>

JONATHAN GULLIVER
HIGHWAY ADMINISTRATOR

PATRICIA A. LEAVENWORTH, P.E.
CHIEF ENGINEER



Dear Concerned Citizen:

The Massachusetts Department of Transportation (MassDOT) is committed to building and maintaining a transportation infrastructure that is both safe and efficient for all who use our roadways, bridges, bicycle facilities and pedestrian paths, while maintaining the integrity of the environment.

As part of the design process for this project, we are conducting this public hearing to explain the proposed improvements, listen to your comments and answer any questions you may have. At the conclusion of the hearing, MassDOT will review all of your comments and, where feasible, incorporate them into the design of the project.

We recognize that road and bridge construction can create inconveniences for the public. MassDOT places a great deal of emphasis on minimizing the temporary disruptive effects of construction.

MassDOT encourages input from local communities and values your opinions. Please be assured that we will undertake no project without addressing the concerns of the community.

Sincerely,

Patricia A Leavenworth, P. E.
Chief Engineer

WHAT IS A PUBLIC HEARING?

WHY A PUBLIC HEARING?

To provide an assured method whereby the Commonwealth of Massachusetts can furnish to the public information concerning the State's highway construction proposals, and to afford every interested resident of the area an opportunity to be heard on any proposed project. At the same time, the hearings afford the Commonwealth an additional opportunity to receive information from local sources which would be of value to the State in making its final decisions to what design should be advanced for development.

WHY NOT A VOTE ON HIGHWAY PLANS?

The hearings are not intended to be a popular referendum for the purpose of determining the nature of a proposed improvement by a majority of those present. They do not relieve the duly constituted officials of a State highway department of the necessity for making decisions in State highway matters for which they are charged with full responsibility.

WHAT DOES A PUBLIC HEARING ACCOMPLISH?

It is designed to ensure the opportunity for, or the availability of, a forum to provide factual information which is pertinent to the determination of the final alternative considered by the state to best serve the public interest, and on which improvement projects are proposed to be undertaken.

It is important that the people of the area express their views in regard to the proposal being presented, so that views can be properly recorded in the minutes of the meeting. These minutes will be carefully studied and taken into consideration in the determination of the final design.

RIGHT OF WAY ISSUES

A secure right of way is necessary for this project. Temporary construction easements may be required. Your municipality is responsible for acquiring all necessary rights in private or public lands. If your property is affected, your rights are fully protected under law.

1. REASON FOR PROJECT

The completion of this project will serve local needs. The proposed enhancement will also be in the interest of others in the greater community, and provide for the public good.

2. WHO CONTACTS ME?

Representatives of the municipality have already contacted or will contact you. They will explain the procedures used in acquiring any necessary rights in land.

3. WHAT ABOUT DONATIONS? WHAT IS A RIGHT OF ENTRY?

Town officials will often seek donations, of parcels, where permanent rights are required. This procedure will minimize the acquisition cost for your community.

A Right of Entry is a document that is signed by the owner. It allows the Contractor to perform certain types of work on the owner's land. The work is usually minor in nature and frequently consists of loaming/seeding behind sidewalks, new driveway apron work, grading/sloping, and wetland protection, etc. The rights granted are temporary in nature.

4. WHAT IS A FAIR PRICE FOR THE ACQUIRED PARCELS?

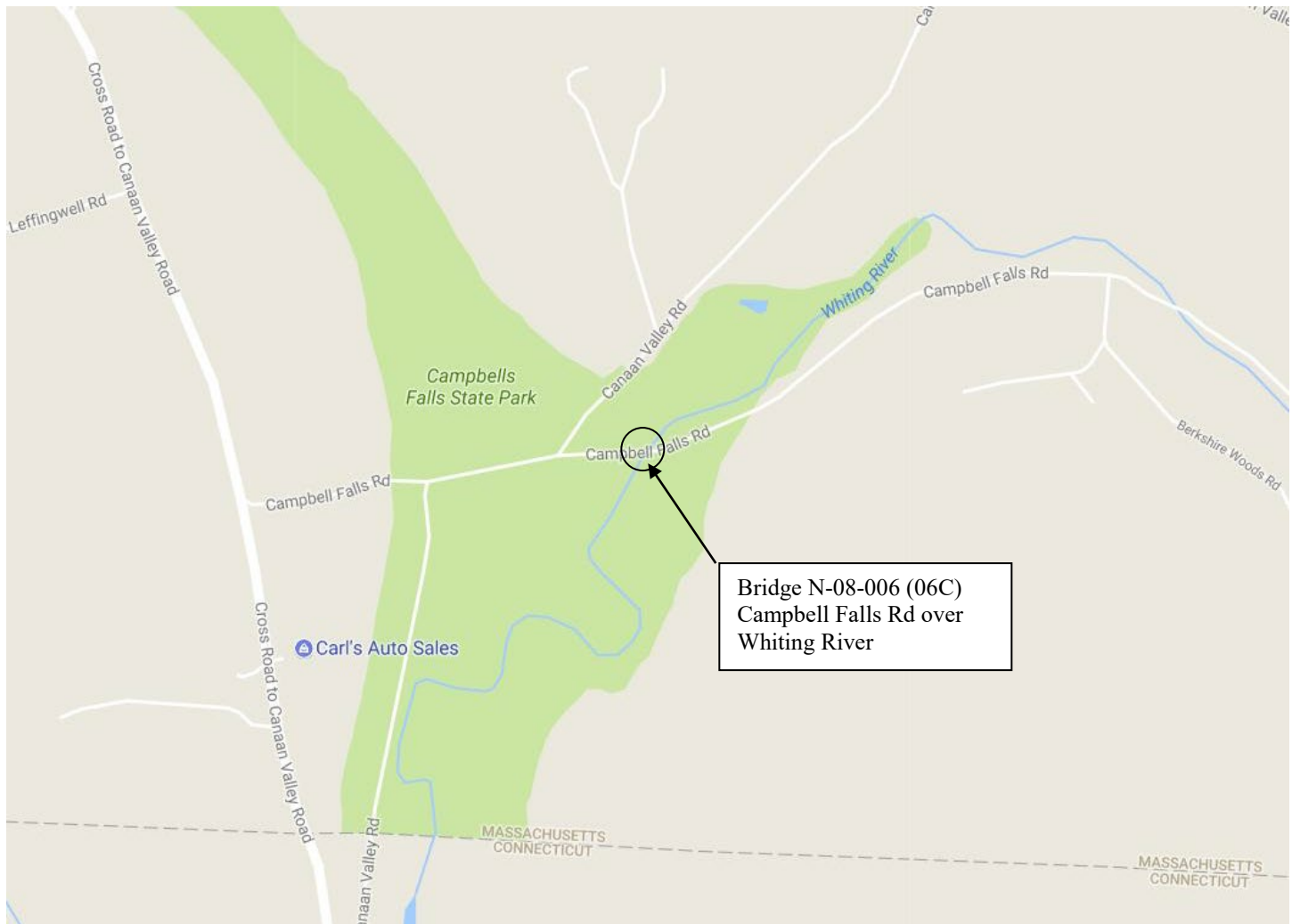
In the event that donations are not considered, or completed, every effort will be made to ensure that an equitable value is awarded. Municipal and/or outside appraisers will complete an appraisal. Consideration is given to the type of rights needed, whether in fee, permanent or temporary easements. The appraisal will be the basis for arriving at a fair price (for damages that result).

5. MUST I ACCEPT THE MUNICIPALITY OFFER?

No, if the owner feels that the offer is not fair the owner may petition the courts. This action does not stop or delay the acquisition. The action must occur within 3 years. The owner(s) may be paid pro tanto (for the time being). The pro tanto payment will not prejudice the court's final decision.

DESIGN PUBLIC HEARING

Bridge No. N-08-006 • Campbell Falls Road over Whiting River
New Marlborough, MA • August 2019



LOCUS MAP
N.T.S.

BRIDGE RECONSTRUCTION/REHAB PROJECT

PROJECT NO. 608645

Campbell Falls Rd over Whiting River, Bridge No. N-08-006

PROJECT LOCATIONS

The N-08-006 (06C) bridge carries Campbell Falls Road, over the Whiting River in New Marlborough, MA. Campbell Falls Road travels east and west. The bridge is due east of Canaan Valley Road. The proposed work limits include improvements to the approach roadways that extend approximately 175-ft to the west and 155-ft to the east of the bridge, for a total project length of approximately 375-ft.

PURPOSE

The purpose of this hearing is to provide the public with the opportunity to become fully acquainted with the bridge rehabilitation project in New Marlborough. All views and comments made at the hearing will be reviewed and considered to the maximum extent possible.

EXISTING CONDITIONS

Campbell Falls Road is a single-lane road classified as a Rural Local Road. The roadway approaches to the bridge are approximately 16-ft wide asphalt surfaces between with no sidewalks. The existing bridge width is 18-ft with a clear width of 14-ft from curb to curb. The roadway is bordered by concrete curbs with metal railings on both sides. Because of additional jersey barriers that were added along the south curb, the horizontal clearance has been reduced to about 12'-0".

The existing bridge was built in 1938 to carry Campbell Falls Rd over the Whiting River. The single span superstructure consists of a concrete deck supported by steel stringers. The deck terminates at the front face of the abutment backwalls. The superstructure is supported on concrete gravity abutments. Splayed concrete wingwalls are located at each corner of the abutments. The overall length of the bridge is 42- ft 2 1/4-in.

Existing bridge plans indicate a concrete deck with a thickness that varies from 6" at the curbs to 7 1/2" at the crown. The deck is covered with gravel up to 3". The structure does not carry any existing utilities.

The bridge is currently posted for truck weight restrictions.

The bridge is located horizontally along a tangent section of roadway. Vertically, the bridge is located on crest curve with downgrades of approximately 5.7% west of the bridge and 1.4% east of the bridge. The existing speed limit within this stretch of roadway is 25 miles per hour. The roadway carries approximately 120 vehicles per day.

PROPOSED IMPROVEMENTS

The proposed bridge will be a single span bridge and the existing abutments will be reused with modifications. The roadway width will match existing on the approaches of 16 ft. The proposed bridge will have a wider clear width than existing, of 16' to match the proposed approaches. The proposed 42-ft 2 1/4-in total length matches existing. Other than the widening of the bridge, there are no significant geometric changes to the horizontal geometry. The bridge profile will be raised in order to keep the bridge seat elevation the same and provide haunches, a thicker deck, and a wearing surface. The proposed vehicular bridge railing will be a CT-TL2.

The proposed bridge will provide about 1 foot of freeboard between the hydraulic design flood water surface elevation and the proposed superstructure low chord (as applicable). The Hydraulic Report, generated by MassDOT, concluded that both the existing and proposed structures are hydraulically adequate and will safely convey the 10-year design storm.

The work on each approach will consist of reconstruction of the existing bituminous pavement, minor vertical alignment improvements, and safety improvements such as proper guardrail transitions to the bridge. The limits of approach work are approximately 175-ft west of, and 155-ft east of the bridge.

UTILITIES

There are overhead wires that exist north of the bridge.

RIGHT OF WAY IMPACTS

There are no right of way impacts.

ENVIRONMENTAL

The project, as currently designed, will result in impacts to Waters of the United States, thereby necessitating the preparation of a Section 401 Water Quality Certificate and a Section 404 Army Corps of Engineers (ACOE) Applications to be filed with the Massachusetts Department of Environmental Protection and ACOE with MassDOT as the proponent.

MAINTENANCE OF TRAFFIC DURING CONSTRUCTION

The bridge will be closed during construction and traffic will be detoured. Strategic sequencing of the work will minimize the duration of the bridge closures.

PROJECT COST

The total preliminary construction cost estimate for the entire project is just under \$800,000.

PROJECT STATUS

The design plans presented here represent the 25% Design Level. Comments made at this public hearing will be incorporated to the maximum extent feasible in the final design documents. The project is scheduled to be advertised in the Summer of 2020 and construction duration is anticipated to extend into the Late 2021.

THE COMMONWEALTH OF MASSACHUSETTS
MASSACHUSETTS DEPARTMENT OF TRANSPORTATION
HIGHWAY DIVISION

New Marlborough, MA
Bridge Reconstruction/Rehab Project
Project File No. 608645

This sheet is provided for your comments. Your input is solicited and appreciated. Please return your sheet, with comments, to a staff member at the meeting, or mail to:

Patricia A. Leavenworth, P.E., Chief Engineer
MassDOT – Highway Division
10 Park Plaza, Boston, MA 02116-3973
Attn: Bridge Project Management

The final date for receipt of written statements and exhibits for inclusion into the official hearing transcript will be ten (10) days after the Public Hearing.

PLEASE TYPE OR PRINT LEGIBLY.

Name: _____ Title: _____

Organization: _____

Address: _____

Please Fold and Tape

Please Place
Appropriate
Postage Here

Patricia A. Leavenworth, P.E.
Chief Engineer
MassDOT – Highway Division
10 Park Plaza
Boston, MA 02116-3973

RE: Public Hearing
Bridge Reconstruction/Rehab Project
NEW MARLBOROUGH
Project File No. 608645
Insert section: Bridge Project Management